



## DEVELOPMENT COMMITTEE

6<sup>th</sup> January 2022

Report of the Corporate Director of Place

Classification: Unrestricted

### Application to discharge requirements under a Development Consent Order

[click here for case file](#)

<b>Reference</b>	PA/21/01190
<b>Site</b>	King Edward Memorial Park Foreshore, Glamis Road, Wapping, E1W 3EQ
<b>Ward</b>	St Katharine's & Wapping (with the most northern extent in Shadwell ward)
<b>Proposal</b>	<p>The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014 application to discharge Schedule 3 Requirements:</p> <p>KEMPF2 - Location of permanent works; KEMPF3 - Detailed design approval for permanent above-ground structures; KEMPF4 - Detailed design approval for signature ventilation columns; KEMPF5 - Detailed design approval for river wall and foreshore structure; KEMPF6 - Landscaping works; KEMPF14 - Surface water drainage; and PW11 - Interpretation strategy (project-wide requirement)</p>
<b>Summary Recommendation</b>	Discharge requirements
<b>Applicant</b>	Bazalgette Tunnel Limited
<b>Architect/agent</b>	CVB
<b>Case Officer</b>	Tanveer Rahman
<b>Key dates</b>	<p>Application validated on 21/05/2021 Public consultation finished on 18/08/2021</p>

## EXECUTIVE SUMMARY

This application for proposed works is not submitted under the Town and Country Planning Act 1990, as is the case ordinarily for applications determined at the Development Committee (DC).

Instead, the application seeks approval for details (as referenced in the 'proposal' section above) that are required to be submitted by The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014. These are required to be in accordance with a set of associated parameter plans approved as part of this Development Consent Order (DCO). These agreed plans are: 'Access plan', 'Demolition and site clearance', 'Site works parameter plan' and 'Landscape plan foreshore area'.

What is before the Council and the DC to determine with this application is limited to a set of details that are more akin to planning conditions that might be associated with a Full Planning Permission and to some degree Reserved Matters within the context of an Outline Planning Application.

This application before the DC cannot affect a previously taken decision as part of the DCO to house the following on the site: an existing combined sewer overflow in this location; a storm overflow chamber to manage flow into the Thames after periods of high rainfall; a control kiosk within the existing park; the erection of five ventilation columns within a new foreshore structure extending out into the Thames (which is already in an advanced stage of engineering construction). Rather the current application is limited to the precise location and finalised finish design details of the main foreshore structure, the river wall and associated structures, the landscaping plan, surface drainage and heritage interpretation strategy.

The details submitted are satisfactory in respect of the foreshore structure in relation to the existing river wall, in terms of scour protection, future maintenance, new surface water run-off, safety of rivercraft and managing future rising sea levels due to climate change. Consultation responses received from the Environment Agency and the Port of London Authority (who manage the River Thames) raise no objection to the details submitted.

The built features set at ground level involve a comprehensive landscaping scheme that would include additional benches and bins for park users, additional landscaped areas for people to enjoy (including three stepped intertidal terraces), six model artwork ships providing users with items of historic and artistic interest; totem signage to aid way-finding within the park itself and along the adjacent Thames Path; as well as the large sculpturally shaped ventilation columns which provide further artistic interest alongside serving a vital practical function of safely expelling treated air.

The scheme also involves very much functional elements such as an electrical control kiosk, a new gate on Glamis Road and bollards. Officers conclude that these elements are fabricated of high-quality materials and are suitably designed for their context.

Safety, security and maintenance aspects of the entire proposal have been assessed carefully. Equally concerns surrounding potential amenity impacts on neighbouring properties arising from the location of the kiosk have been successfully addressed in the design detailing.

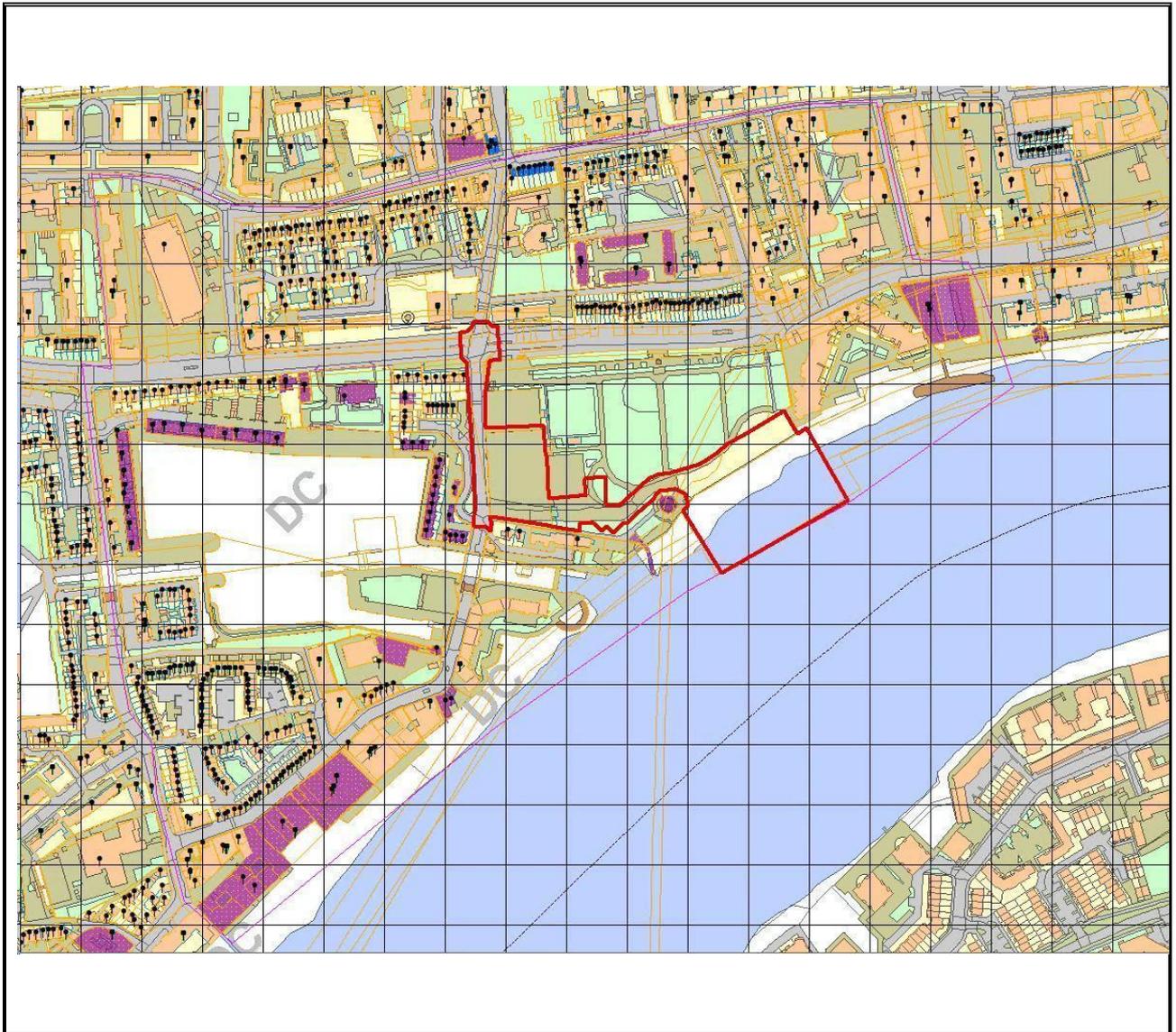
The scheme gives no concern in respect of handling of surface drainage from the new foreshore structure

To conclude, the details submitted for the finished design of the foreshore structure itself, set alongside associated above ground structures and a comprehensive hard and soft landscaping scheme are carefully and imaginatively handled; such that the Thames Tideway foreshore development will serve as an attractive, significant and positive addition to King Edward Memorial Park. The scheme will expand the area of public realm within the park by approximately 8% and

provide an opportunity for people to get closer to the river surface and enjoy three intertidal terraces, with the introduction of a lower terraced path that provides level access to comply with the principles of inclusive design, alongside stepped entrance from the west. The scheme would also improve the experience for those walking along the adjacent Thames Path.

Overall, the development is considered to comply with relevant Requirements of the DCO and its relevant guideline documents.

**SITE PLAN**



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- Planning Application Site Boundary
- Other Planning Applications
- Consultation Area
- Land Parcel Address Point
- Locally Listed Buildings
- Statutory Listed Buildings

**Planning Applications Site Map  
PA/21/01190**

This site map displays the Planning Application Site Boundary and the extent of the area within which neighbouring occupiers / owners were consulted as part of the Planning Application Process



**London Borough  
of Tower Hamlets**

Scale: 50m grid squares

Date: 09 December 2021

## 1. SITE AND SURROUNDINGS

- 1.1 The site is located in Wapping and relates to the western and southern sections of King Edward Memorial Park (KEMP) and the new foreshore structure (KEMPF) itself. KEMPF serves the needs of the Thames Tideway Tunnel which is currently under construction and is surrounded by a temporary cofferdam.
- 1.2 The Thames Path runs through the eastern section of the park. Pedestrian access into KEMP via the Thames Path is currently from the west and the east. There is also access into KEMP from the north west via Glamis Road and the north via The Highway.
- 1.3 Vehicular access for Tideway's contractors to access their temporary construction compound is currently from the south west via Glamis Road.
- 1.4 KEMP is subject to masterplan proposals currently being prepared by the Council that are not controlled by the DCO but will bring about changes and improvements to the park involving: improved facilities for sport and play, enhanced soft landscaping and repairs to the fabric throughout the park. These park improvements will be funded by financial obligations from the Section 106 legal agreement for the DCO.
- 1.5 The site is in the Wapping Wall Conservation Area and a Tier 2 Archaeological Priority Area (Shadwell). The Grade II listed Rotherhithe Tunnel Air Shaft Rotunda is in the southern section of KEMP and the Grade II listed Shadwell Dock Stairs are to the south west of this.
- 1.6 The LBTH Local Plan identifies KEMP as Publicly Accessible Open Space, a Site of Importance for Nature Conservation (SINC) and part of the Council's Green Grid Network.



Figure 1: Aerial view of the site

- 1.7 The nearest residential properties to the site are at Free Trade Wharf (FTW) to the east and Trafalgar Court and Pear Tree Lane to the west.

## **2. BACKGROUND**

### ***The Development Consent Order***

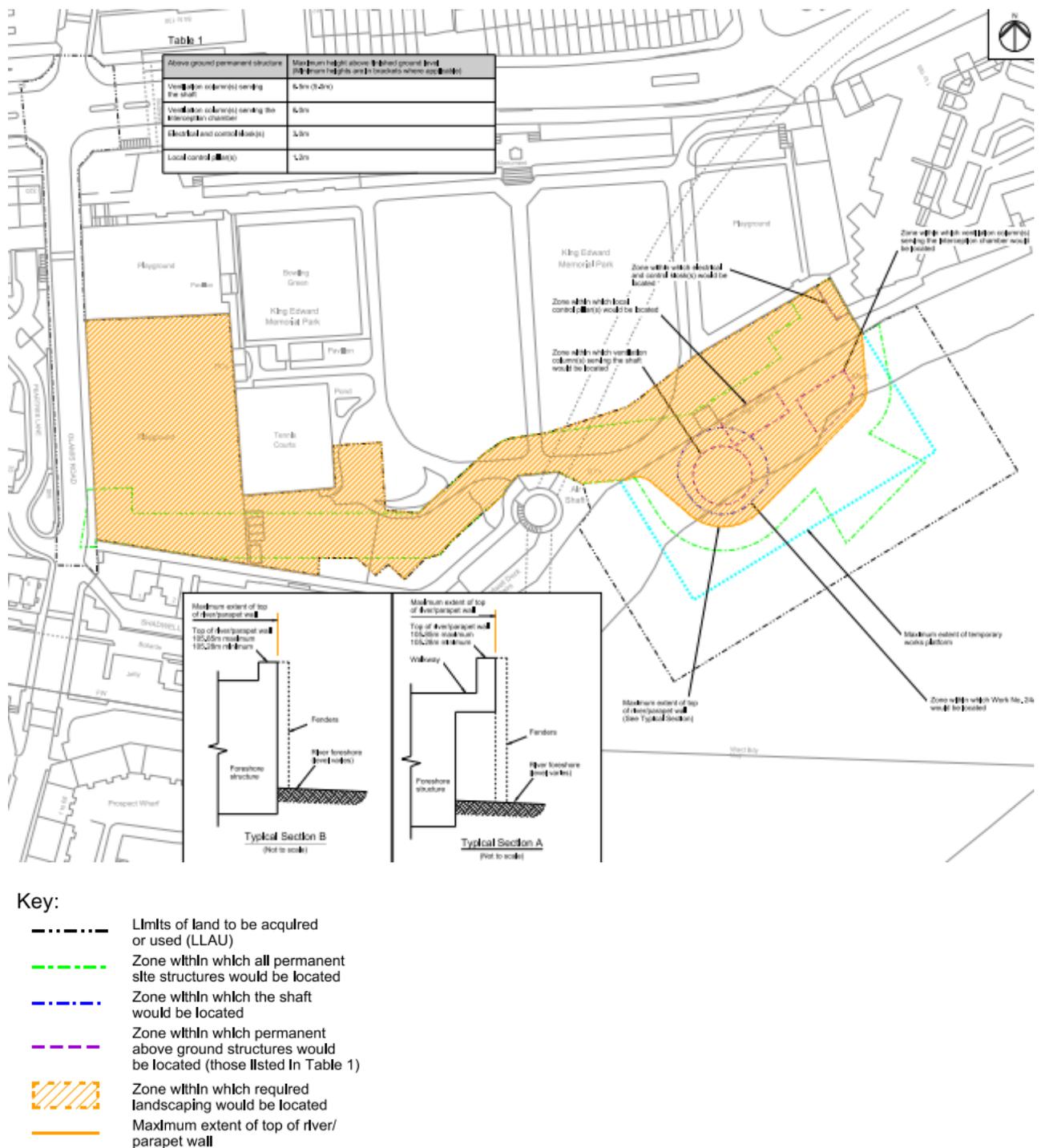
- 2.1 On September 12<sup>th</sup> 2014 The Secretary of State approved the Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014. This DCO granted permission for the Thames Tideway Tunnel which is a Nationally Significant Infrastructure Project.
- 2.2 The Thames Tideway Tunnel is a 25km long 'super sewer' which would serve London. Its purpose is to capture, store and transfer sewage for treatment at Beckton Sewage Treatment Works. This is to alter the current situation of sewage flowing directly into the River Thames when rainfall volumes exceed the capacity of Joseph Bazalgette's 19<sup>th</sup> Century London sewage system.
- 2.3 There are 24 Thames Tideway Tunnel sites, running from Acton Storm Works in the west to Beckton Sewage Treatment Works in the east. KEMPF is one of these sites.
- 2.4 The DCO contains requirements for further detailed information to be submitted via applications during different stages of the development.
- 2.5 It also contains the legislative framework for submission of these applications by Tideway and assessment of them by the Local Planning Authority (LPA) and other relevant consultees.
- 2.6 Of particular relevance to this current application is Schedule 3 of the DCO. This sets out a number 'Requirements' for detailed information to be submitted to and approved by the LPA (in consultation with relevant consultees) prior to the construction of above ground works and landscaping.

### ***KEMPF***

- 2.7 The existing North East Storm Relief (NESR) combined sewer overflows (CSO) currently discharges approximately 782,000m<sup>3</sup> of untreated sewage into River Thames in front of KEMP. The NESR CSO discharges approximately 31 times a year and releases 200 tonnes of sewage derived litter. The KEMPF works are required to connect the NESR CSO to the main tunnel.
- 2.8 The functional purpose of the KEMPF structure is to house a CSO drop shaft, a storm overflow chamber to allow the CSO to flow into the Thames after periods of high rainfall, a chamber and louver chamber for ventilation control, an air treatment unit and other hydraulic structures which would all be underground.

### ***Relevant legislation***

- 2.9 Schedule 2, Part 4 of the DCO requires above ground permanent works at KEMPF to be in accordance with the following approved parameter plans:
  - Demolition and site clearance - DCO-PP-24X-KEMPF-250004
  - Access plan - DCO-PP-24X-KEMPF-250003 – rev 1
  - Site works parameter plan - DCO-PP-24X-KEMPF-250005 - rev 1 (*See Figure 2 below*)
  - Landscape plan foreshore area - DCO-PP-24XKEMPF-250009 rev 1



**Figure 2: Site works parameter plan - DCO-PP-24X-KEMPF-250005 - Rev 1 (approved under the 2014 DCO)**

- 2.17 There are eighteen Schedule 3 requirements specific to the KEMPF site; as well as a further nineteen which relate to all of the sites along the Thames Tideway Tunnel route. The application before the Committee is to discharge six specific KEMPF conditions and one project wide condition.
- 2.18 The Section 106 agreement for the KEMPF site sets out the responsibilities for the phasing and maintenance of the park going forwards. Part 2 of the agreement requires Tideway to serve notice on the Council on or before the construction phase completion date, identifying “Permissive Public Realm” (land for public use). Although, it does list situations where this may need to be temporarily

suspended i.e. safety and essential maintenance reasons. Part 3 of the agreement requires Tideway to submit a strategy for long-term maintenance of the “Permissive Public Realm” to the Council on or before the construction phase completion date. It goes on to state that maintenance is the responsibility of Tideway or a third party that they delegate to.

2.19 However, access to KEMP (including the KEMPF structure) will continue to be controlled by the LBTH Parks & Open Spaces team.

### **3. PROPOSED DEVELOPMENT – DISCHARGE OF SCHEDULE 3 REQUIREMENTS**

3.1 This current application seeks to discharge the following seven Schedule 3 requirements:

- KEMPF2 - Location of permanent works
- KEMPF3 - Detailed design approval for permanent above-ground structures
- KEMPF4 - Detailed design approval for signature ventilation columns
- KEMPF5 - Detailed design approval for river wall and foreshore structure
- KEMPF6 - Landscaping works
- KEMPF14 - Surface water drainage
- PW11 - Interpretation strategy (project-wide requirement)

3.2 The submission contains drawings and details for a new area of public realm that would serve as an extension to the park. The comprehensive landscaping details were prepared so they are consistent with the agreed principles of the Council co-ordinated masterplan for the whole park.

3.3 The proposal would consist of functional structures that are required by the DCO and have their locations and dimensions outlined in the DCO approved parameter plans. This would consist of:

- Two signature vortex and three non-signature ventilation columns which expel air that has been cleaned and had odours removed by underground passive filters.
- An electric/control kiosk to help control and manage the control sewage overflows and related functional activities.

3.4 The DCO and the associated approved parameter plans also require preparation of a comprehensive landscaping scheme involving hard and soft landscaped areas, drainage and public art. In response to these requirements the applicant’s submission includes:

- Hard and soft landscaping areas within the existing park which are within the relevant limits. (*See Figure 3 below*)
- Hard and soft landscaping areas within the KEMPF structure. This would incorporate different levels which would be fully accessible via ramp. It would also incorporate three stepped intertidal terraces which contain planting and would be floodable.
- A new river wall.
- Public art consisting of six bronze art piece ships set on individual plinths and arranged on a trail.
- Surface water drainage.
- Relocation of an existing park bandstand closer towards the foreshore.
- Provision of integrated and standalone benches.
- New bins and cycle stands.
- New fencing, balustrades, handrails and an entrance gate to Glamis Road.
- Two totem signs.
- Two removable bollards.



Figure 3: Proposed site plan showing boundaries between Tideway's relevant limits and LBTH masterplan areas (drawing submitted as part of this current application)

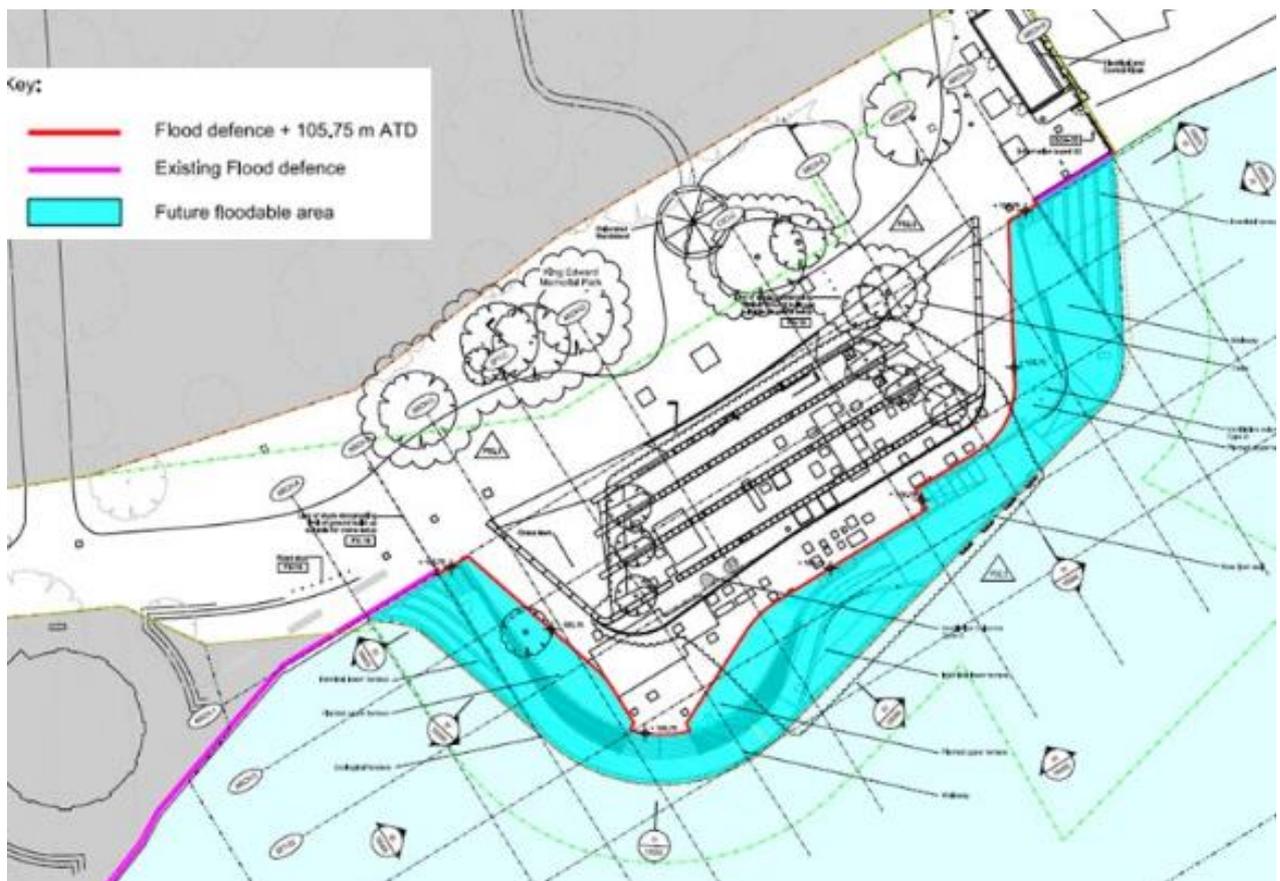


Figure 4: Proposed plan showing future floodable public realm (drawing submitted as part of this current application)

#### 4. RELEVANT KING EDWARD MEMORIAL PARK PLANNING HISTORY

##### *Tideway*

- 4.1 **PA/14/03672:** Provision of Thames Tideway Tunnel. Permitted 12.09.2014 and S106 signed 16.01.2015
- 4.2 **PA/16/03100:** The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014: Application to partially discharge Schedule 3 Requirement PW6 (CoCP Part A) - Community Liaison Plan. Permitted 16.12.2016
- 4.3 **PA/16/03096:** The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014: Application to discharge Schedule 3 Requirement PW10 – Signage for Temporary footpath diversions. Permitted 06.01.2017
- 4.4 **PA/16/03131:** The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014: Application to discharge Schedule 3 Requirement KEMPF7 - Works to reconfigure the multi-purpose sports pitch, relocate the children's play area and associated landscaping. Permitted 12.01.2017
- 4.5 **PA/17/01827:** The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014 Application to partially discharge Schedule 3 Requirement PW15 (River Transport Strategy) - Sustainable Freight Transport Plan. Permitted 17.10.2017
- 4.6 **PA/19/01906:** The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014 Formal application under Schedule 3 to partially discharge KEMPF5 (River Wall Construction - Works Requirement 24b (iv)) Submission Ref: C415-KEMPF-219. Permitted 13.09.2019
- 4.7 **PA/20/02414:** Draft application under the Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014 seeking comments on proposal to discharge the following Schedule 3 requirements: • KEMPF2: Location of Permanent Works • KEMPF3: Detailed design approval for permanent above-ground structures • KEMPF4: Detailed design approval for signature ventilation columns • KEMPF5: Detailed design approval for river wall and foreshore structure • KEMPF6: Landscaping works • KEMPF14: Surface water drainage • PW11: Interpretation strategy (project-wide requirement). LPA response issued 18.02.2021

##### *Other*

- 4.8 **PA/14/03236:** Proposal to widen existing gate to allow easier access to vehicles. The development would also widen an existing brick ramp, on the park side of the development site. Permitted 09.02.2015

#### 5. PUBLICITY AND ENGAGEMENT

- 5.1 Given the scale and nature of the proposal as well as the level of local interest in the scheme, it has been decided by the Corporate Director of Place and the Development Manager that this application should be determined at the Development Committee (DC), as per Section 5A of the referral criteria for DC.
- 5.2 Thames Tideway have also been involved in extensive community engagement surrounding their proposals for KEMP and KEMPF going back to 2017 including the establishment of a Community Liaison Working Group (CLWG).
- 5.3 A structured public consultation event is not a formal requirement of the DCO. However, a consultation event relating to this current application was held by the applicant Tideway on

December 15<sup>th</sup> 2020 as part of the draft application PA/20/02414, with Officers from the Council present. The event was held online due to COVID-19 restrictions. Comments made at the event from the public and other consultees were set out in the LPA's subsequent formal written response to Tideway.

- 5.4 For this current application public consultation by the LPA is not a formal requirement under the DCO. However, notifications were undertaken via letters to 2,324 individual addresses, a press notice, site notices and text on the LBTH website; as set out in paragraphs 5.4 - 5.6 below.
- 5.5 A press notice was published on June 10<sup>th</sup> 2021 and three site notices were displayed next to the application site on July 19<sup>th</sup> 2021.
- 5.6 The application was also publicised on an LBTH Planning & Building Control webpage and an LBTH Parks & Open Spaces webpage.
- 5.7 38 letters of objection from 35 individuals/couples (including a letter on behalf of the Turk's Head Charity) were received for this current application.
- 5.8 These comments can be summarised as follows:

#### *Procedure*

- The application should not be determined under delegated authority.
- The current application has lacked a public consultation event.
- The Council has not individually notified residents of this application.
- Neighbour notification letters arrived late.

#### *Previous consultations*

- Tideway have dismissed most of the community feedback

#### *Accessibility of walkways*

- Access to the riverside walkway is impeded by steps from the west so is not inclusive and accordingly should be rejected.

#### *General design*

- The park needs to stay true to its original intention as a facility for the people.
- The foreshore structure is too large and prominent.
- The design does not improve sightlines to the river as stated in the submission.
- The scheme should provide a public toilet and baby changing facilities, otherwise existing issues that arise from an absence of a toilet in the park will be exacerbated.
- There is no provision to improve the amenity for the many dog owners who use the park.
- The scheme appears to involve loss of heritage railings.
- Concerns that mound seats and concrete edges to the benches could be used by skaters.
- The bins are at odds with the rest of the park's aesthetic.
- The scheme would impede views of the park and therefore devalue property prices.
- The Glamis Road gate is defensive, overbearing and does not relate well to the park's existing boundary.
- The submission does not address the eastern entrance to the park from the Thames Path.
- In terms of sightlines, the balustrading is an obstacle to river views.

### *Hard and soft landscaping*

- There is too much hard landscaping and not enough soft landscaping.
- The access road would be an unattractive feature and truncate the proposal from the rest of the park, rather than its stated aim of integrating into it.
- Tideway have not demonstrated that the maintenance tasks justify the need for the road.
- The scheme would involve loss of trees and lacks details of a management plan for trees.
- Visuals of the trees do not appear to tie in with the schedule.

### *Ventilation shafts*

- The structures are ugly, look dated and are not in keeping with the park's Edwardian character.
- CLWG are disappointed repeated calls over the years to change the colour and materials for the two vent columns have been ignored.
- At a minimum all the columns on the two mounds should be the same material/colour.

### *Kiosk*

- The structure is utilitarian in appearance, out of character with the Edwardian park, would be an eyesore and is poorly sited located close to a boundary wall and peoples' homes. It would pose amenity, safety and security issues to residents in FTW, as it would make it easier for intruders to scale the boundary wall and over their fencing.
- The dimensions are larger than approved at the public inquiry.
- A brick finish would be more in keeping and liable to weather better.
- The green roof will inevitably die and affect the appearance too.
- The design of the existing Victorian ventilation shaft provides clues to the sort of architecture that fits into area.
- The design falls short of a project associated with Bazalgette and in an area associated with Brunel.
- The kiosk could potentially become a magnet for noise, anti-social behaviour and attract graffiti.
- Tideway should provide a guarantee that the kiosk cannot be used to gain access to FTW.
- The application lacks detail on what hazards will be inside the kiosk, including flammable gases or liquids.
- An extractor fan would face FTW. If the output of the extractor is noxious or harmful it should be relocated.
- It should be located underground.
- The space behind the kiosk could attract rubbish and vermin; raising questions over the proposed cleaning regime.
- It involves the loss of trees which is not acceptable.
- It is set too close to the gate, creating a dangerous pinch point for pedestrians.

### *Artwork*

- Not in keeping with the park, nor conducive to the enjoyment of the park.
- Represents cultural appropriation.
- Focus on emotive issues that are unlikely to assist with community cohesion.
- The art fails to relate to the history of the park, to the heritage of the dock communities or to Captain Cook who lodged locally.
- Money would be better spent on mitigating road traffic noise or compensating residents affected by construction works.
- Little evidence feedback from community consultation has informed the art strategy work.
- More community engagement from the appointed artist was sought.
- The artworks should be better integrated within the main park and tie in with the wider masterplan for the park - so as to act as way markers to the river.

### *Boundary treatments*

- The proposal does not tie in with the avenue of Leylandii trees along the south western boundary (next to the Shadwell Basin Outdoor Activity Centre).
- The proposed access road for maintenance vehicles fails to tie in with the appearance and pedestrian usage of the parallel Thames Path.

### *Other*

- From the outset the opinions of residents from FTW have been ignored and rejected.
- Residents have been subject to repeated noise and disruption, including an incident which resulted in the overnight evacuation of residents from 25 flats.
- The construction timetable is ever lengthening.
- Agreed extended working hours have increased local disruption.
- Concerns that construction related noise and vibration could exacerbate health issues.
- Concerns with the design and park security, especially at night and if at a future date the Council choose to no longer lock the park at night
- Would the scheme provide connections to electricity and water for temporary uses and events?

5.9 A letter of objection from Councillor Golds was received which made the following comments:

- The structure is 30cm higher than the boundary wall with FTW, making it easy for intruders to gain access into flats within FTW. The structure should be relocated to address this.
- Queried why St Katharine's and Wapping Safer Neighbourhoods Team had not commented on the application.
- Cllr Golds objects until safety concerns of FTW residents have been resolved.

5.10 3 other representations not in support or against the scheme raised additional matters:

- Need for more TFL cycle hire facilities, as there is a lack in the area.
- Hope that certain historic elements to the foreshore including a stone causeway and wooden wharfs are incorporated into the design.
- Seek enhancements to the park through introduction of a true variety in the tree species introduced to replace lost trees and seek an exploration of wilding to encourage greater biodiversity.
- Dorothea Smartt's site-specific poems linked to London's lost rivers should be incorporated, as other Tideway sites have done.

## **6. CONSULTATION RESPONSES**

### **Internal consultees**

#### **LBTH Place Shaping**

- 6.1 Following clarification of materials during the application process, LBTH Place Shaping Officers are satisfied with the appearance, safety and maintenance aspects of the submission. This includes the five ventilation columns, a comprehensive hard/soft landscaping scheme, artwork plinths with ships, the Glamis Road gate, balustrading, benches, bins, bollards and totem signage.
- 6.2 The general utilitarian appearance of the kiosk is noted. However, given that its location and size parameters are set by the DCO and the fact that it has functional requirements that are key to the operation of the Thames Tideway Tunnel, its overall design and materiality is on balance considered to be modest in appearance and acceptable in terms of appearance, safety and maintenance.

6.3 LBTH Place Shaping therefore raise no objection.

#### **LBTH Environmental Health - Odours**

6.4 No objection

#### **LBTH Environmental Health - Air Quality**

6.5 No objection.

#### **LBTH Environmental Health - Contamination**

6.6 No objection.

#### **LBTH Parks & Open Spaces**

6.7 The proposed intertidal terraces are strongly supported for their amenity value and as a tool for bringing people closer to the river.

6.8 All public areas would be accessible via Part M compliant ramps which is also strongly supported.

6.9 The proposed planting and associated maintenance plan are supported.

6.10 A variety of seating has been provided. Some have armrests and over 50% meet BS8300-2:2018 and the DFT's inclusive mobility guidance (2005). This provides seating for people with different abilities and preferences; and is therefore supported.

6.11 There is a constant flux of people along the Thames Path during the day and the proposal offers little seclusion for rough sleeping. Given this good passive surveillance there are no major concerns that any of the proposed elements would encourage anti-social behaviour.

6.12 The proposed metal fixings and notches on street furniture to deter skateboarding is supported.

6.13 No objection to the overall maintenance plan.

6.14 No issues with safety, theft and graffiti of the proposed artwork; subject to the Met Police raising no objection.

6.15 LBTH Parks & Open Spaces therefore support the design proposal which provides a landscaping scheme which is coordinated with the agreed masterplan principle for the wider park.

#### **LBTH Biodiversity**

6.16 No objection.

#### **LBTH Arboriculture**

6.17 No objection.

#### **LBTH Sustainable Drainage Systems (SUDS)**

6.18 Comments are incorporated within the 'Assessment' section of this report.

## **External consultees**

### **Canal & River Trust**

6.19 Stated that they did not wish to comment.

### **Environment Agency (EA)**

6.20 No objection to KEMPF2, KEMPF5, KEMPF6 and KEMPF14

6.21 No comment on KEMPF3, KEMPF4 and PW11 as they are not relevant to the EA's remit.

### **Historic England Greater London Archaeological Advice Service**

6.22 No objection.

### **Historic England**

6.23 No objection.

### **London Fire and Emergency Planning Authority**

6.24 No response received.

### **Port of London Authority (PLA)**

6.25 No objection to KEMPF2, KEMPF 5 and KEMPF6.

6.26 No comment in relation to KEMPF3, KEMPF4, KEMPF14 and PW11 as they fall outside the scope of the PLA's remit.

### **LB Southwark**

6.27 No response received.

### **TFL (City Planning)**

6.28 Stated that technical approval should be sought from the TFL Structures team.

### **TFL (Structures)**

6.29 Stated that they operate a separate consents regime which does not prevent the LPA determining this application.

### **Metropolitan Police - Designing Out Crime Officer**

6.30 No objections in principle.

6.31 Requested a Secured by Design (SBD) Condition.

*(Officer's note: Schedule 3 of the DCO does not allow for conditions to be added to a Requirement. However, an informative will be added inviting Tideway to achieve SBD accreditation before the Council agrees to take over maintenance from Tideway.)*

*An additional informative will be applied to seek that Tideway maintain a continued dialogue with the Met Police in regard to security and fire safety.)*

## **7. RELEVANT LEGISLATION & DOCUMENTS**

7.1 The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014 provides the complete planning and legal framework for the entire Thames Tideway Tunnel development and contains 19 Schedules. Of particular relevance to this application are the following Schedules of the DCO:

- Schedule 1 (The Authorised Project) which lists all the approved works.
- Schedule 2 (Plans) which lists the works plans, land plans, access and approved plans.
- Schedule 3 (Requirements).
- Schedule 17 which deals with the procedure for discharging the requirements.

7.2 Other material considerations for assessing applications are the following documents published by Thames Water/Tideway:

- Design Principles Dated 11 March 2014 Ref: APP206.01LL
- PUBLIC ART STRATEGY ART ON THE TIDEWAY: TURNING TO FACE THE RIVER (PAS)
- HERITAGE INTERPRETATION STRATEGY 'RIVER OF LIBERTY' FULL REPORT (HIS)

7.3 Beyond the Thames Tideway Tunnel Order the following background documents have relevance with respect to informing an assessment of the scheme:

- National Planning Policy Framework (2021)
- Planning (Listed Buildings and Conservation Areas) Act 1990
- Listed Buildings and Curtilage Historic England Advice Note 10 (2018)
- Tower Hamlets Local Plan 2031 (2020)
- LBTH Wapping Wall Conservation Area Character Appraisal and Management Guidelines (2009)

7.4 As stated in Section 2 above of this report, the DCO granted permission for the Thames Tideway Tunnel. This included the principle of the KEMPF site; as well as the outline locations of the foreshore structure and the below ground and above ground structures (including the size and location of the kiosk and the height of the ventilation columns) contained within it. These outline details are contained in the following approved DCO drawings:

- Demolition and site clearance - DCO-PP-24X-KEMPF-250004
- Site works parameter plan - DCO-PP-24X-KEMPF-250005 - rev 1

7.5 This current application is seeking approval for more detailed information which will be assessed in Section 8 below.

## **8. PLANNING ASSESSMENT**

8.1 The following will be assessed as part of this application:

1. KEMPF2
2. KEMPF3
3. KEMPF4
4. KEMPF5
5. KEMPF6
6. KEMPF14
7. PW11
8. Human Rights and Equalities

## **KEMPF2 - Location of permanent works**

### Requirement

- 8.2 This Requirement seeks details of the exact extents of the KEMPF structure (i.e. the outline of the new river wall and intertidal terraces).
- 8.3 Part (1) of this requirement states that “*The permanent foreshore structure and river wall shall not extend beyond the alignment shown on the Site works parameter plan*”.
- 8.4 As the foreshore structure does not extend to the permitted alignment (see figure 2 above), part (2) of this requirement is engaged which states that “*Should the alignment be less than the maximum extent shown on the Site works parameter plan, details of the amended alignment, which shall accord with the design principles for this site, shall be submitted to and approved by the relevant planning authority in consultation with the Port of London Authority and the Environment Agency*”.

### Assessment

- 8.5 The EA has assessed the application in terms of the KEMPF structure’s levels being high enough to accommodate future rises in river levels. The EA has deemed this issue to have been successfully addressed and therefore raises no objection to the discharge of KEMPF2.
- 8.6 The PLA has assessed the application in terms of the KEMPF structure having sufficient scour protection to protect the river wall and also the impacts on the safety of river users. The PLA has deemed these issues to have been successfully addressed and therefore raises no objection to the discharge of KEMPF2.
- 8.7 For these reasons Officers consider that KEMPF2 has been addressed.

## **KEMPF3 - Detailed design approval for permanent above-ground structures**

### Requirements



**Figure 5: Proposed illustrative CGI bird's eye perspective view of KEMPF structure (at low tide) from the south west (submitted as part of this current application)**

- 8.8 Part (1) of this requirement states that “*Construction of any permanent above-ground structure shall not commence until details of the design (including size, external appearances and materials), which shall accord with the design principles for this site and the Site works parameter plan, are submitted to and approved by the relevant planning authority*”.
- 8.9 The location and maximum size parameters of the kiosk were set by the DCO (12.5m width x 5m depth x 3m height) and the Design Principles document required a minimum 0.8m separation distance from the FTW boundary wall. Therefore, the kiosk would need to comply with these size and locational requirements. Its overall appearance also needs to be considered acceptable.
- 8.10 The DCO sets the height of the non-signature ventilation columns to be 6.0m. It also sets out the eastern zone in which they could be located.

### Assessment

#### *Kiosk*

- 8.11 The kiosk would be 11.5m wide x 2.223m deep x 2.923m high and would be set 0.878 - 1.02m from the boundary wall and railings with FTW to the east. It therefore complies with the locational and size parameters of the DCO approved drawings and the Design Principles document.
- 8.12 Officers note that its footprint would be larger than that contained in the DCO approved drawing ‘Landscape plan foreshore area’. However, that drawing is illustrative in landscaping terms and does not prevent the kiosk’s size from extending to the locational and size parameters set out in that drawing as well as in another DCO approved drawings ‘Site works parameter plan’.
- 8.13 Officers note concerns raised by residents about the kiosk’s appearance. It is recognised that its rectangular form and its elevational treatment consisting of large louvred doors, concrete with vertical timber cladding and brick walling would give it a somewhat utilitarian appearance. However, Officers also recognise that the size has already been established and that functional requirements give rise for the need for large openings and a robust structure.
- 8.14 Some design thought has been given to its appearance by the selection of timber fins. Furthermore, the proposed timber has been selected to be hard-wearing and to blend into its context rather than to stand out. The living roof is considered to be a positive element. Overall, it is concluded the external appearance of the kiosk is acceptable.
- 8.15 Officers note concerns raised by some residents about security, the prospect of accumulation of waste between the eastern elevation of the kiosk and the FTW boundary wall/railings and matters of graffiti and vandalism.
- 8.16 Two approximately 2.3m high fences would close off the area to the rear (east), whilst presently this space is open with existing trees set alongside the boundary wall, providing opportunities to scale the wall and boundary railings. Furthermore, the vertical timber fins and 2.923m height of the kiosk would make it difficult for individuals to climb on top of the kiosk and the secured gap to the eastern boundary wall would further discourage bids to use the kiosk as a means to attempt to scale the boundary wall. With respect to graffiti the Council’s Parks & Open Spaces Team have commented that the fins are less likely to attract graffiti than a regular flat surface.
- 8.17 In regard to general maintenance and concerns over littering to the rear of the kiosk and more generally Thames Tideway have confirmed in their Hard and Soft Landscape maintenance plan that Thames Water will be responsible for the area behind the kiosk and that the in place S106 agreement requires a strategy for the long-term maintenance and care of the public realm to be submitted to the Council.

- 8.18 Paying due regard to comments from the Met Police and the Council's Parks & Open Spaces Team, neither of whom have objected to the application, Officers are of the view that there are no design aspects or features of the kiosk's design which could reasonably be considered to increase the potential for intruders to access to FTW, litter or undertake graffiti/vandalism.
- 8.19 Officers note concerns raised by some residents about amenity impacts on FTW residents.
- 8.20 During the application process, Officers sought clarification from Tideway on the intended usage of the kiosk. Tideway stated that there are likely to be weekly "house-keeping visits", which would involve visual inspection, occasional cleaning and calibration of instruments and minor repairs if needed. This would generally last no more than a couple of hours to complete. Occasionally this visit would be combined with one of the quarterly or 6-monthly scheduled maintenance works on one of the systems and in this case the visit could last a whole day. They added that major maintenance operations (yearly or less) would occur where the operational area is fenced off for a few days would also include some access to the kiosk. Finally, external access to the living roof would be required up to four times a year, and that these would rarely be outside of normal working hours.
- 8.21 Tideway also confirmed that the extract fan in the east elevation is purely to ventilate the interior space and would therefore only expel hot air which would not be toxic in any way.
- 8.22 Officers conclude that the kiosk's use and maintenance would not give rise to any undue or unacceptable impacts to neighbours in terms of noise or pollution.
- 8.23 For the reasons set out above it is considered that overall the kiosk's size, external appearance and materiality are acceptable.

#### *Non-signature ventilation columns*

- 8.24 The three non-signature ventilation columns would have a maximum height of 6m and would be located within the eastern zone set out in the parameter plans approved as part of the DCO. They are therefore considered to comply with the locational and size parameters of the DCO.
- 8.25 They would have a more linear and simplistic form than the two proposed vortex signature vortex columns. However, they are considered to work well as group of three to provide a counterpoint to the vortex columns.
- 8.26 The warm greyish brown/bronze tone finish of the columns is considered to work well with their size, location and form; contributing to the development having a warm feel which would work well within the wider park context.
- 8.27 For these reasons it is considered that overall the size, external appearance and materiality of the three ventilation columns are acceptable.

#### **KEMPF4 - Detailed design approval for signature ventilation columns**

##### Requirements

- 8.28 The DCO sets the height of the signature ventilation columns to be a minimum of 5.0 and a maximum of 6.5m. It also sets out the western zone in which they could be located.
- 8.29 Part (1) of this requirement states that "*Construction of the ventilation columns shall not commence until details of the height, dimensions, external appearance and materials, which shall accord with the design principles for this site are submitted to and approved by the relevant planning authority in consultation with the HBMCE*".

### Assessment

- 8.30 The two signature ventilation columns would have a maximum height of 5.4m and would be located within the western zone set out in the parameter plans approved as part of the DCO. They are therefore considered to comply with the locational and size parameters of the DCO.
- 8.31 The twisted form of the two vortex columns is considered to create an interesting sculptural form which would add visual interest to the foreshore area.
- 8.32 The columns will be cast in a foundry and constructed of cast iron that will provide an attractive degree of variation in the general finish appearance. Towards the base of columns a distinct and very much bespoke pattern will be introduced into the finish of the columns. This patterning on their base, inspired by the nearby Rotunda's grilles, are considered to be an attractive feature that work well with their size and location and add further visual interest to the form. These signature columns will share the same warm greyish brown/bronze tone colour tone and finish as the other columns.
- 8.33 The finish design of the columns is imaginative and successful and serve as good counterpoint to the other ventilation columns in the development and visually work well within the new foreshore structure in terms of relating to the wider park context.
- 8.34 For these reasons and paying due regard to comments from HE, LBTH Place Shaping and the LBTH Parks & Open Spaces Team, Officers consider that the detailed design of the signature ventilation columns would be a positive feature within the landscape of the park and in views from the Thames Path and the river.

### **KEMPF5 - Detailed design approval for river wall and foreshore structure**

#### Requirements

- 8.35 This requirement relates to the proposed KEMPF structure and its associated river wall which would surround it.
- 8.36 Part (1) of this requirement requires *"details of the design (including external appearance and materials), which shall accord with the design principles for this site and the Site works parameter plan"* to be *"submitted to and approved by the relevant planning authority in consultation with the Environment Agency in respect of land based access to flood defences and potential for ecological enhancements"*

#### Assessment

- 8.37 This requirement has some overlap with the requirements of KEMPF2.
- 8.38 Following the increase of the gap from the existing river wall to the western intertidal terrace of 0.3m (to aid any future monitoring and repair) and the Council confirming that they understand the potential additional costs of repair to the river wall that the terraces could create, the EA raise no objection to the discharge of KEMPF5.
- 8.39 Therefore, Officers consider that the detailed design for the river wall and foreshore structure is acceptable.

## **KEMPF6 - Landscaping works**

### Requirements

- 8.40 Part (1) of this requirement sets out that construction of above-ground structures cannot commence until landscaping details (excluding those for KEMPF7 (reconfiguration of the MUGA and child's play area and associated landscaping) in line with the Proposed Landscape Plan and Site Works Parameter Plan approved as part of the DCO and the Design Principles document have been approved by the LPA.
- 8.41 Part (2) goes on to state that unless otherwise agreed by the LPA these hard and soft landscaping details should be submitted to include:
- a) Location, quantity, species, size and density of any proposed planting.
  - b) Cultivation, importation of materials and other operations to ensure plant establishment.
  - c) Monitoring and maintenance (including any maintenance or restoration of landscaping required after tunnel commissioning).
  - d) Proposed finished ground levels.
  - e) Hard-surfacing materials.
  - f) Minor structures such as furniture, refuse or other storage units, signs and lighting.
  - g) Retained historic landscape features and proposals for restoration, where relevant.
  - h) A programme for implementation of all landscaping works.
  - i) Details of fencing/enclosures.
  - j) vehicular and pedestrian access, parking and circulation areas, (including details of areas of public access).
  - k) Proposed and existing functional services above and below ground, including drainage, power and communications cables and pipelines, manholes and supports.
  - l) Details of existing trees to be retained.
- 8.42 Based on these requirements hard and soft landscaping needs to be considered.
- 8.43 Furthermore, the location, size and appearance of the relocated existing park bandstand, integrated benches, stand-alone benches, bins, cycle stands, fencing, balustrades, handrails, entrance gate from Glamis Road, totem signage and removable bollards need to be considered as well.

### Assessment

- 8.44 The proposed soft landscaping is dictated heavily by necessary underground and overground infrastructure as well as the access road. The following five planting zones would be created:
- 1. Foreshore planters (trees, herbaceous perennials, grass and shrubs)
  - 2. Bank planter and grassed Mound (trees, Herbaceous perennials, grass and shrubs)
  - 3. Central seated areas trees (herbaceous perennials)
  - 4. Interface with existing park (trees, turf, shrubs and a green roof)
  - 5. Intertidal terraces (intertidal plants)
- 8.45 The 2014 DCO permitted 29 trees to be felled in order to facilitate the construction site. A further 4 trees were permitted to be felled under Article 27 of the DCO from 2016 - 2017.
- 8.46 This application proposal would involve the planting of 54 replacement trees. This is a net increase in trees which is in accordance with the Design Principles document.

### *Hard and soft landscaping*

- 8.47 Officers note concerns raised by some residents that the amount of hard surfacing compared to soft landscaping would make the park appear heavily urbanised and uninviting.
- 8.48 Officers understand these concerns. However, it is noted that the approved underground infrastructure (which includes the CSO drop shaft with its large diameter, air treatment chamber, valve chamber, interception chamber and North East Storm relief sewer) all dictate that much of the ground above would need to consist of hard surfacing (including maintenance hatches). Furthermore, Officers recognise the operational need for the hard surfaced road north of the KEMPF structure for access by serving vehicles.
- 8.49 The soft landscaping would consist of grass and plants around the proposed path, the KEMPF structure and within the three proposed intertidal terraces.
- 8.50 The Biodiversity Officer raises no objection to the proposed tree and planting species and noted that the planting contributes to Biodiversity net gain and serves several objectives and targets in the Tower Hamlets Local Biodiversity Action Plan.
- 8.51 The LBTH Senior Arboricultural Officer supports the number and species of the proposed trees. Following clarification from Tideway, the Officer is also satisfied with the submitted planting and maintenance methodologies.
- 8.52 In conclusion, based upon the comments received from the Biodiversity Officer, the Senior Arboricultural Officer about the soft landscaping and comments also received from the Place Shaping Officer and the Parks & Open Spaces Team it is concluded the approach and level of detail submitted on hard and soft landscaping is positive, will provide for a new open space of high-quality to serve the wider park and views and enjoyment of the River Thames. Officers particularly note that the proposed intertidal terraces are an innovative and positive feature in terms of public amenity value, providing opportunities for all visitors to get close to the river itself as well as providing interesting planting opportunities.

### *Other structures*

- 8.53 The standalone benches and bins would be of traditional design which are already in use in LBTH parks. Their appearance is accordingly appropriate and supported. Officers welcome their location along a main path in terms of contributing more widely to users of the park.
- 8.54 The Glamis Road gate would have a steel vertical fin structure. Officers consider that its contemporary design provides an unobtrusive contrast with the Edwardian Park's existing railings.
- 8.55 The relocated bandstand would be to the north of the KEMPF structure and accessible by paths to the north and south and provides a fitting location for such a structure. Its traditional appearance will remain as before and is in keeping with the Edwardian park.
- 8.56 The integrated benches, cycle stands, balustrading, totem signage and movable bollards would provide a more contemporary appearance to the existing park. However, these elements are located within the main KEMPF structure; and the design intention of this element is to be more contemporary. Officers are supportive of this approach, which gives visual clues to the new underground infrastructure below. Officers consider the design of these elements to be of a high quality, appropriate to their specific location in the park and whilst contemporary in appearance will visually complement and integrate with the existing park.

### *Other matters*

- 8.57 Officers note comments made by local residents requesting toilet/baby changing facilities and an off-leash area for dogs. These facilities are not a requirement of the DCO. Officers therefore do not consider it necessary or reasonable for them to be provided by Tideway. It is worth noting the Council-led masterplan for the park does currently include proposals for a commercially run cafe with toilets.

### **KEMPF14 - Surface water drainage**

#### Requirement

- 8.58 Part (1) of this requirement states that “*Construction of the permanent above-ground structures or landscaping shall not commence until details of the surface water drainage system for this site (including means of pollution control, an assessment of the hydrological and hydrogeological context and how the scheme shall be maintained and managed following completion), which shall accord with the design principles for this site, are submitted to and approved by the relevant planning authority in consultation with the Environment Agency.*”

#### Assessment

- 8.59 During the application process LBTH SUDS requested the following further information: records of stated approvals for stats drainage and SUDS, surface water discharge calculations or a site-specific suds assessment, a completed LBTH SUDS Proforma, confirmation of finished flood defence wall heights and a maintenance regime for the SUDS scheme and a Flood Risk Assessment (FRA).
- 8.60 These comments were relayed to the applicant. In response the applicant provided details demonstrating that an FRA and sections covering water resources and flood risk were part of the Environmental Statement submitted as part of the Environmental Impact Assessment assessed by the SOS for the original DCO application.
- 8.61 In response to this LBTH SUDS raised no objection.
- 8.62 The EA has also raised no objection.
- 8.63 To conclude and in summary the proposed surface water drainage is acceptable.

### **PW11 - Interpretation strategy (project-wide requirement)**

#### Requirement

- 8.64 *Part (1) of this requirement states that “A project-wide heritage interpretation strategy shall be developed in consultation with the HBMCE within 12 months of the start of construction, in accordance with the OAWSI and design principle HRTG.07”.*

*(Officer’s note: ‘OAWSI’ means the Overarching Archaeological Written Scheme of Investigation Ref: APP195)*

- 8.65 The Design Principles document adds further guidance on this requirement by stating that “*A project-wide interpretation strategy shall be developed to celebrate the pioneering nature and significance of Bazalgette’s sewerage system, and the engineering achievements of the project as a sensitive development of London’s historic sewer system. This shall take account of any existing local interpretation strategies. The design of interpretative materials at the site level shall be sensitively integrated into the design of the new facilities and surrounding area and avoid creating unacceptable visual clutter*”.

8.66 The proposed six artwork ships therefore need to be considered based on these requirements.

#### Assessment

8.67 The proposed artwork ships would consist of a Tug, Chinese Junk, Flat Barge, Carrack, Thames Barge and a Bangla River Boat. They would be cast in Bronze by the distinguished artist Hew Locke, based on the submitted drawings and indicative images in the submitted 'Appendix C King Edward Memorial Park - proposed heritage interpretative artwork' and would form a trail within the limits permitted by the DCO.

8.68 Tideway's Public Arts Strategy (PAS) states that its project vision is to "*reconnect London, and Londoners, back with the River Thames*". The guidelines in the Heritage Interpretation Strategy (HIS) for the PW11 requirement goes on to state that it "*should not be over-literal or too concerned with an 'accurate' reflection of the 'past'. Whilst rooted in the historic cultural narratives, representations incorporated in the design should be capable of multiple readings and a plurality of meanings*".

8.69 Officers note comments from some residents about the relevance of these proposed artwork ships to the history of the local area.

8.70 The selected ships draw upon connections and have taken influence from communities that have migrated to the local and wider area of the Wapping waterfront. Officers consider this to be an acceptable selection, in accordance with guidelines in the HIS.

8.71 Officers recognise that artwork is subjective and that many different approaches could have been taken. However, Officers consider that the selected ships satisfy the PAS approach in not seeking to be too literal in the approach taken to its reflecting upon the past, but rather are of a design that lend themselves well to being capable of multiple readings and to offer a plurality of meanings to people.

8.72 The general size and appearance of the six proposed bronze statues set on their plinths is considered appropriate in the park context.

8.73 Comments made by some residents stating that the ships could be located more widely through the park to create a trail are noted. Officers have no objection to that idea. However, it is understood why Tideway have chosen to keep these artworks within extent of works set out and permitted by the parameter plans of the DCO. Officers have assessed the artwork proposals on their own merit and on this basis the location of the plinths is found to be acceptable.

8.74 Officers note that neither LBTH Parks & Open Spaces nor the Met Police have raised any objection with their location or design.

8.75 For these reasons Officers consider that on balance the proposed artwork ships would satisfy the Interpretation Strategy for the KEMPF site.

#### **Human rights & equalities**

8.76 The proposal does not raise any unique human rights or equalities implications. The balance between individual rights and the wider public interest has been carefully considered and Officers consider it to be acceptable.

8.77 Officers are satisfied that the proposed development would not result in adverse impacts upon equality or social cohesion.

## **9. CONCLUSION**

- 9.1 Officers assessed the submitted details against the relevant sections of the DCO and guidance in Design Principles document, the Public Art Strategy and Heritage Interpretation Strategy. Officers have also had due regard to the NPPF (2021), Planning (Listed Buildings and Conservation Areas) Act 1990, Listed Buildings and Curtilage Historic England Advice Note 10 (2018), the LBTH Local Plan 2031 (2020) and the LBTH Wapping Wall Conservation Area Character Appraisal and Management Guidelines (2009). The submitted details are considered to be acceptable in terms of the seven Schedule 3 Requirements that have been applied for; and it is recommended that they should be discharged.

### **Informatives**

1. The applicant is advised to continue to liaise with the Metropolitan Police's Designing Out Crime Officers to achieve Secured By Design Accreditation.
2. The applicant is advised to gain independent third party certification from a manufacturer to ensure the fire performance of any door-sets is in compliance with Building Regulations and accords with the advice issued by the Department for Communities and Local Government on 22nd June 2017.

## **APPENDIX A: APPROVED DOCUMENTS & DRAWINGS**

### **Documents**

- Document Register - 5700-CVBJV-KEMPF-151-TZ-OL-007030 P06
- Appendix B - Tideway Heritage Interpretation Strategy
- Appendix C - KEMPF Proposed Heritage Interpretative Artwork
- Appendix D - KEMPF Specification for Soft Landscape Works
- Appendix E - Planting Schedule - 5600-MOTMA-KEMPF-590-LZ-DC-150002 P03.1
- Appendix E: Images of planting species - 5700-CVBJV-KEMPF-590-LZ-DJ-007290-P01
- Appendix F - KEMPF Community Consultation December 2020
- Appendix H - Design Evolution May 2021
- Appendix I - Tideway's response to neighbour comments from the draft submission - 5700-CVBJV-KEMPF-151-TZ-CO-007214-P01
- LBTH Response to Draft Submission (PA/20/02414)
- Supporting statement - 5700-CVBJV-KEMPF-151-TZ-RG-006717-P02
- GATE 4 KEMPF hard and soft landscape maintenance and management plan - 5600-CVBJV-KEMPF-590-LZ-RG-007241 P02
- HR Wallingford Detailed Scour Reports Detailed Scour Assessment Doc Ref: 9.09.06
- HR Wallingford fluvial modelling of permanent works at KEMPF – 5600-MOTMA-KEMPF-520-VZ-RG-150004 P02
- KING EDWARD MEMORIAL PARK FORESHORE Additional Information - 5700-CVBJV-KEMPF-151-TZ-EN-007836-P01
- KEMPF PERMANENT WORKS: SUMMARY OF RESIDENTS RESPONSES - 5700-CVBJV-KEMPF-520-TZ-EN-005063-P01
- Schedule of materials - 5700-CVBJV-KEMPF-151-TZ-EN-007502 P04
- Additional Information - Tree Removals T16, 23, 56 and 61 - 5700-CVBJV-KEMPF-151-TZ-EN-007905-P01
- Agent e-mail Dated August 10<sup>th</sup> 2021 re: kiosk maintenance
- Agent e-mail Dated October 26<sup>th</sup> 2021 re: benches
- Agent e-mail Dated November 30<sup>th</sup> 2021 re: ventilation columns material
- Bronze artwork sample material photographs

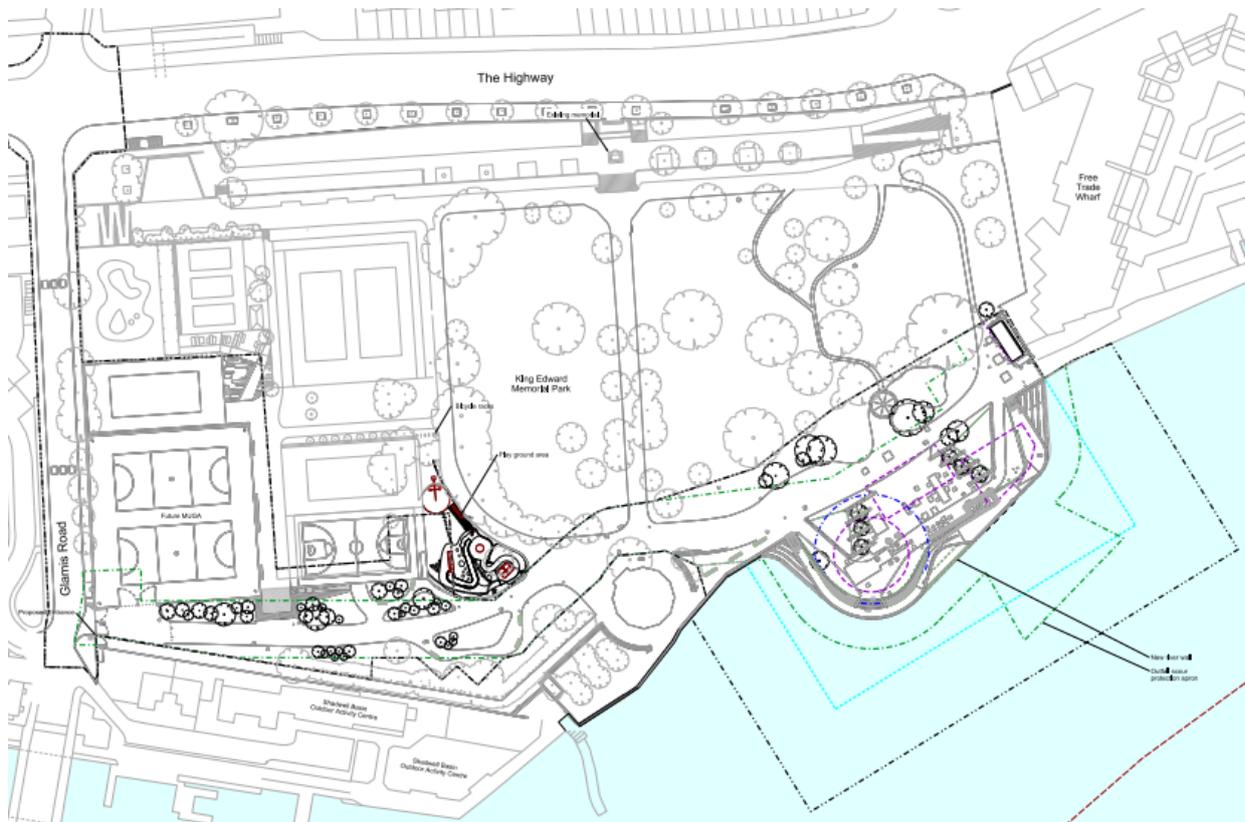
### **Drawings**

- Site works parameter plan - DCO-PP-24X-KEMPF-250005 - Rev 1 March 2014
- KEMPF maintenance plan - 5600-MOTMA-KEMPF-610-ZZ-PQ-150001 – P04
- Location plan - 5600-CVBJV-KEMPF-150-ZZ-DR-550001 P01
- Site block plan - 5600-CVBJV-KEMPF-150-ZZ-DR-550002 P01
- Permanent works foreshore layout plan - 5600-CVBJV-KEMPF-150-ZZ-DR-550003 P01
- Foreshore north and south Elevation - 5600-CVBJV-KEMPF-150-ZZ-DR-550004 P01
- Foreshore east and west elevation - 5600-CVBJV-KEMPF-150-ZZ-DR-550005 P01
- Foreshore section A-A - 5600-CVBJV-KEMPF-150-ZZ-DR-550006 P01
- Foreshore section B-B - 5600-CVBJV-KEMPF-150-ZZ-DR-550007 P01
- Foreshore section C-C - 5600-CVBJV-KEMPF-150-ZZ-DR-550009 P01
- Bandstand plan, section and elevation - 5600-CVBJV-KEMPF-150-ZZ-DR-550012 P01
- Glamis Road layout plan - 5600-CVBJV-KEMPF-150-ZZ-DR-550015 P01
- Glamis Road elevation and section - 5600-CVBJV-KEMPF-150-ZZ-DR-550016 P01
- Glamis Road plan 1 of 3 - 5600-CVBJV-KEMPF-150-ZZ-DR-550017 P01
- Glamis Road plan 2 of 3 - 5600-CVBJV-KEMPF-150-ZZ-DR-550018 P01
- Glamis Road plan 3 of 3 - 5600-CVBJV-KEMPF-150-ZZ-DR-550019 P01
- Central mound elevations - 5600-CVBJV-KEMPF-150-ZZ-DR-550020 P01
- Central mound sections - 5600-CVBJV-KEMPF-150-ZZ-DR-550021 P01

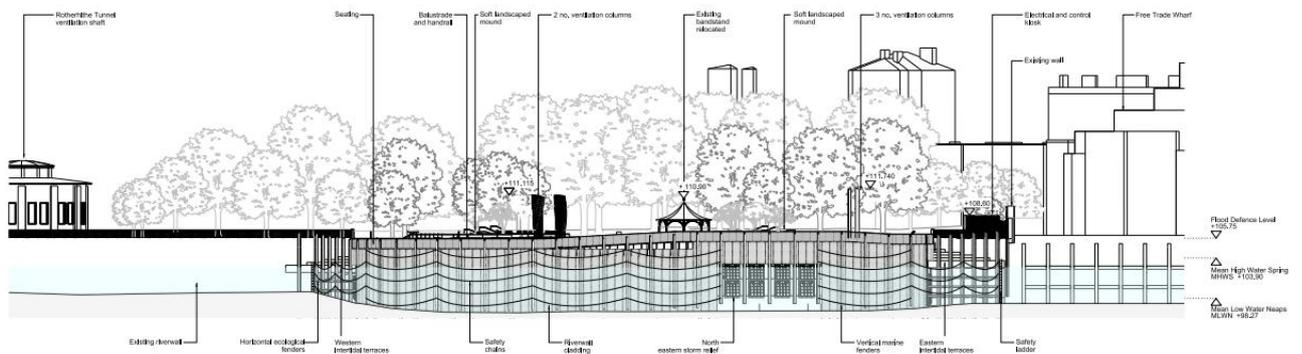
- Western mound - 5600-CVBJV-KEMPF-150-ZZ-DR-550022 P01
- Eastern mound - 5600-CVBJV-KEMPF-150-ZZ-DR-550023 P01
- Kiosk elevations - 5600-CVBJV-KEMPF-150-ZZ-DR-550025 P02
- Kiosk front Elevation and section - 5600-CVBJV-KEMPF-150-ZZ-DR-550026 P02
- Kiosk floor plan and roof plan - 5600-CVBJV-KEMPF-150-ZZ-DR-550027 P01
- Electrical and control kiosk fence and gate - 5600-CVBJV-KEMPF-150-ZZ-DR-550029 P01
- Western mound ventilation column plan and section - 5600-CVBJV-KEMPF-150-ZZ-DR-550030 P01
- Western mound ventilation column details - 5600-CVBJV-KEMPF-150-ZZ-DR-550031 P01
- Eastern mound ventilation column plans and elevations - 5600-CVBJV-KEMPF-150-ZZ-DR-550032 P01
- Shaft & chambers cover slab plan western side - 5600-CVBJV-KEMPF-150-ZZ-DR-550033 P02
- Shaft & chambers cover slab plan eastern side - 5600-CVBJV-KEMPF-150-ZZ-DR-550034 P01
- Soft landscape general arrangement and plan references 1 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550035 P01
- Soft landscape general arrangement and plan references 2 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550036 P01
- Soft landscape detail reference plan 1 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550037 P01
- Soft landscape detail reference plan 2 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550038 P01
- Soft landscape detail reference plan 3 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550039 P01
- Soft landscape detail reference plan 4 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550040 P01
- Soft landscape detail reference plan 5 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550041 P01
- Tree & grass planting plan 1 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550042 P01
- Tree & grass planting Plan 2 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550043 P01
- Tree & grass planting Plan 3 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550044 P01
- Tree & grass planting Plan 4 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550045 P01
- Tree & grass planting Plan 5 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550046 P01
- Bird and bat box location plan 1 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550047 P01
- Bird and bat box location plan 2 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550048 P01
- Kiosk brown roof details - 5600-CVBJV-KEMPF-150-ZZ-DR-550049 P01
- Lower walkway east bench - 5600-CVBJV-KEMPF-150-ZZ-DR-550055 P01
- Benches 1 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550057 P01
- Benches 2 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550058 P01
- Riverwall balustrade details - 5600-CVBJV-KEMPF-150-ZZ-DR-550059 P01
- Riverwall balustrade details 2 of 3 - 5600-CVBJV-KEMPF-150-ZZ-DR-550060 P01
- Riverwall balustrade details 3 of 3 - 5600-CVBJV-KEMPF-150-ZZ-DR-550061 P01
- Intertidal terrace gate and ladder - 5600-CVBJV-KEMPF-150-ZZ-DR-550062 P01
- Intertidal terrace ladder - 5600-CVBJV-KEMPF-150-ZZ-DR-550063 P01
- Glamis Road Gate - 5600-CVBJV-KEMPF-150-ZZ-DR-550065 P01
- Artistic plinth 1 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550067 P01
- Artistic plinth 2 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550068 P01
- Memorial Benches Glamis Road Approach - 5600-CVBJV-KEMPF-150-ZZ-DR-550069 P01
- Litter bin, bollard and information totem - 5600-CVBJV-KEMPF-150-ZZ-DR-550070 P01
- Bicycle racks, life buoy and stud location - 5600-CVBJV-KEMPF-150-ZZ-DR-550071 P01
- Western stair - stair 01 - 5600-CVBJV-KEMPF-150-ZZ-DR-550072 P01
- Central stair - stair 02 - 5600-CVBJV-KEMPF-150-ZZ-DR-550073 P01
- Eastern stair - stair 04 - 5600-CVBJV-KEMPF-150-ZZ-DR-550074 P01
- Central graded route 01 and Stair 03 - 5600-CVBJV-KEMPF-150-ZZ-DR-550075 P01
- Central graded route 01 - 1 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550080 P01
- Central graded route 01 - 2 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550081 P01
- Eastern graded route 02 - 5600-CVBJV-KEMPF-150-ZZ-DR-550082 P01

- Lower walkway graded route 03 - 5600-CVBJV-KEMPF-150-ZZ-DR-550083 P01
- Foreshore mounds axonometric - 5600-CVBJV-KEMPF-150-ZZ-DR-550084 P01
- Riverwall panel elevations, sections & plan 1 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550101 P01
- Riverwall panel elevations, sections & plan 2 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550103 P01
- Riverwall cladding elevations and sections 1 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550104 P01
- Riverwall cladding elevations and sections 2 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550105 P01
- Riverwall cladding elevations and sections 3 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550106 P01
- Riverwall cladding elevations and sections 4 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550107 P01
- Riverwall cladding elevations and sections 5 of 5 - 5600-CVBJV-KEMPF-150-ZZ-DR-550108 P01
- Western intertidal terrace cladding - plan and elevation - 5600-CVBJV-KEMPF-150-ZZ-DR-550113 P02
- Eastern intertidal terrace cladding - plan and elevation 5600-CVBJV-KEMPF-150-ZZ-DR-550114 P01
- Fenders cladding - 5600-CVBJV-KEMPF-150-ZZ-DR-550122 P01
- Lower walkway seating details 1 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550124 P01
- Lower walkway seating details 2 of 2 - 5600-CVBJV-KEMPF-150-ZZ-DR-550125 P01
- Flood defence strategy - 5600-CVBJV-KEMPF-150-ZZ-DR-550126 P01
- Location plan with PLA Chart - 5600-CVBJV-KEMPF-150-ZZ-DR-550127 P01
- Proposed surface water drainage GA plan sheet 1 of 3 - 5600-MOTMA-KEMPF-420-DZ-DA-154020 P03
- Proposed surface water drainage GA plan sheet 2 of 3 - 5600-MOTMA-KEMPF-420-DZ-DA-154021 P03
- Proposed surface water drainage GA plan sheet 3 of 3 - 5600-MOTMA-KEMPF-420-DZ-DA-154022 P03
- Maintenance and repair layouts sheet 1 of 7 - 10 year maintenance - 5600-MOTMA-KEMPF-610-ME-DA-156101 P02
- Maintenance and repair layouts sheet 2 of 7 - replacement and removal of carbon filter and pressure relief dampers - 5600-MOTMA-KEMPF-610-ME-DA-156102 P02
- Maintenance and repair layouts sheet 3 of 7 - replacement and removal of flap valves, penstock and secondary isolation gates - 5600-MOTMA-KEMPF-610-ME-DA-156103 P02
- Maintenance and repair layouts sheet 4 of 7 - opening and maintenance of flap valves, penstocks and secondary isolation gates - 5600-MOTMA-KEMPF-610-ME-DA-156104 P02
- Maintenance and repair layouts sheet 5 of 7 - carbon filters media replacement - 5600-MOTMA-KEMPF-610-ME-DA-156105 P02
- Maintenance and repair layouts sheet 6 of 7 - work site structures commissioning - 5600-MOTMA-KEMPF-610-ME-DA-156106 P02
- Maintenance and repair layouts sheet 7 of 7 - access cover layout - 5600-MOTMA-KEMPF-610-ME-DA-156107 P01
- Interface between western ITT elements and existing river wall - 5600-CVBJV-KEMPF-520-VZ-DR-550001 P01
- Interface between eastern ITT elements and existing river wall - 5600-CVBJV-KEMPF-520-VZ-DR-550002 P01
- Central Graded Route 01 and Stair 03 - 5600-CVBJV-KEM PF-150-ZZ-DR-55007 5 P02

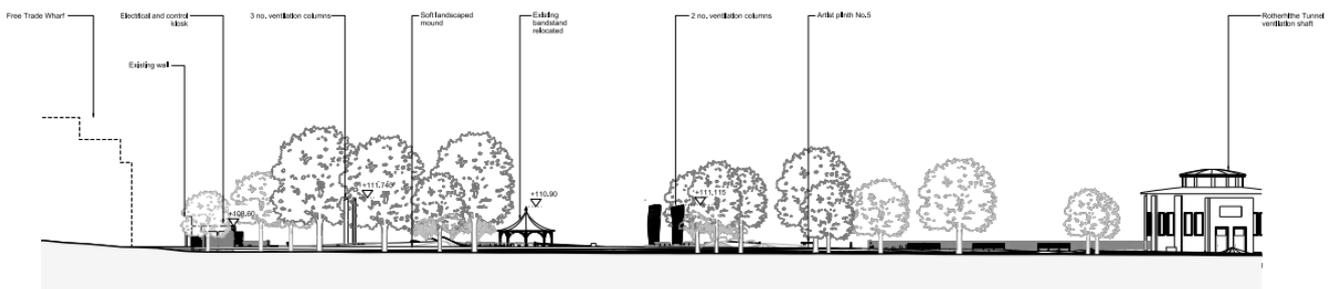
## APPENDIX B: SELECTED DRAWINGS



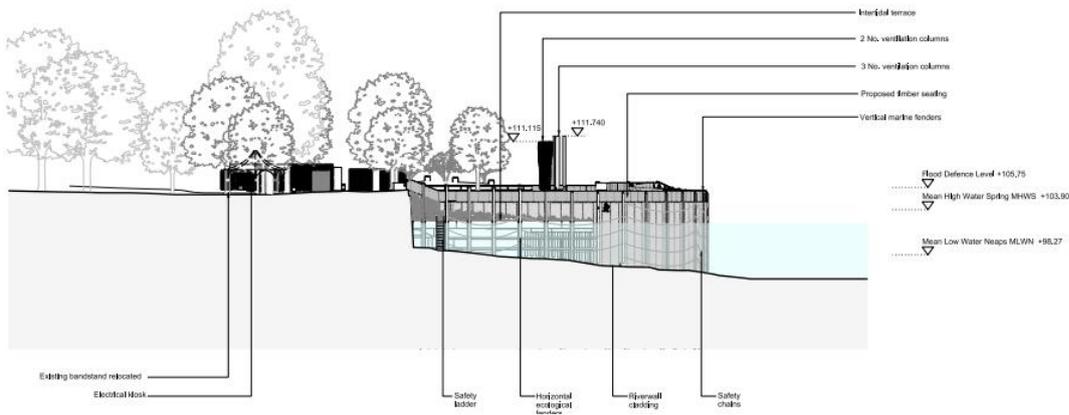
Proposed block plan



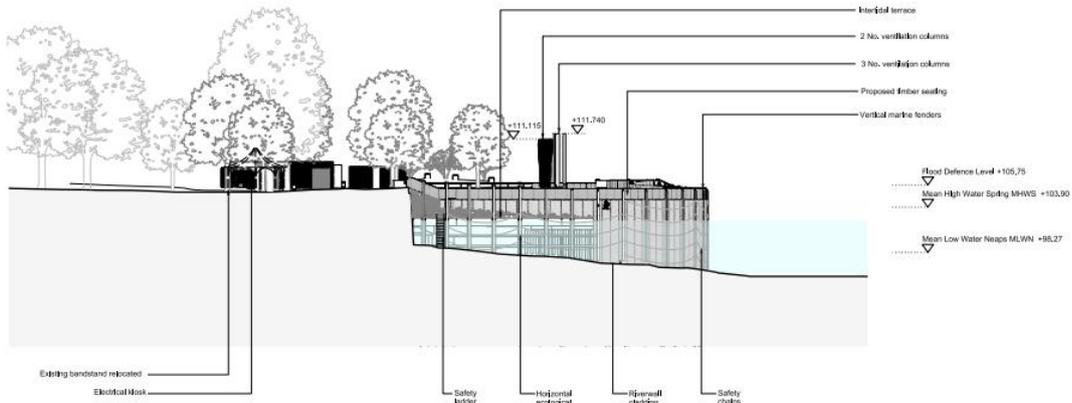
Proposed south elevation



Proposed north elevation

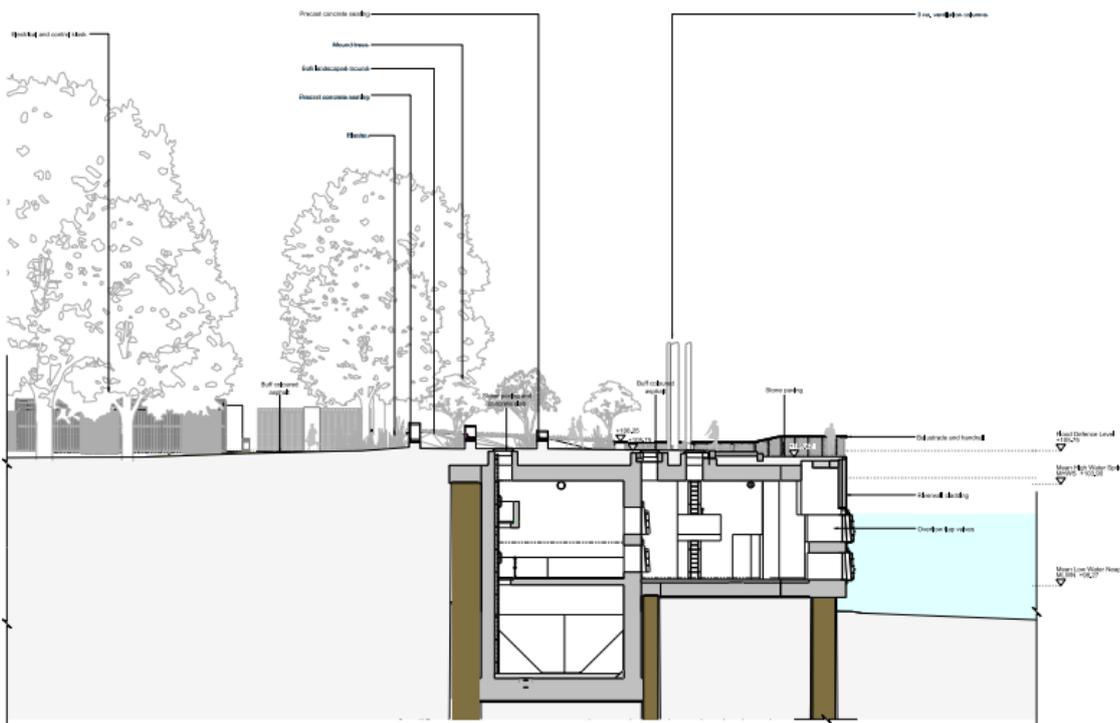


Proposed west elevation



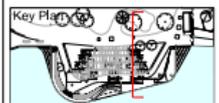
Proposed east elevation

Proposed section



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- Notes:
1. All dimensions are in millimetres unless otherwise stated.
  2. All dimensions and tolerances are as shown, unless otherwise stated.
  3. Check by the building contractor.



First Issue

Checked: LARV

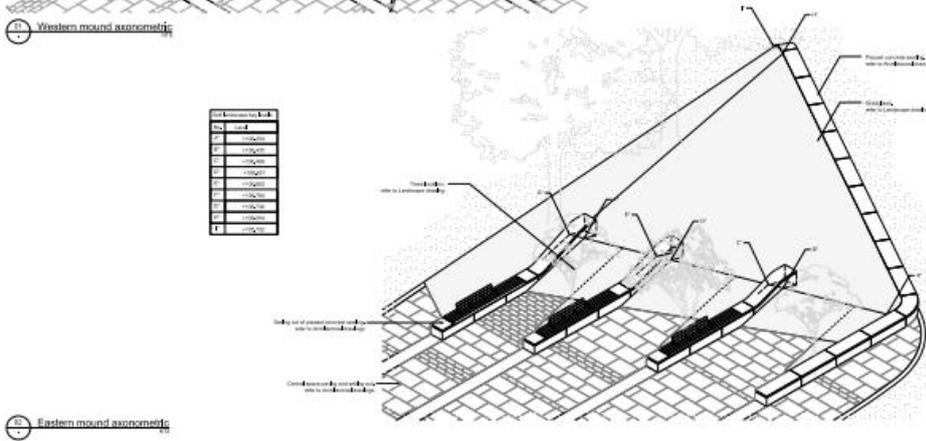
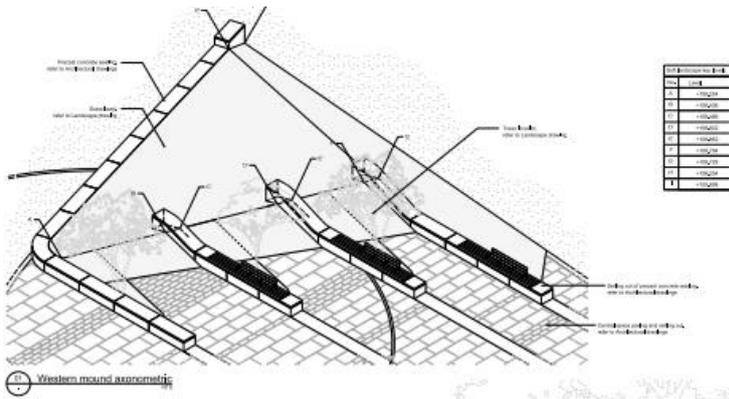
Approved: DLOV

For acceptance: S4

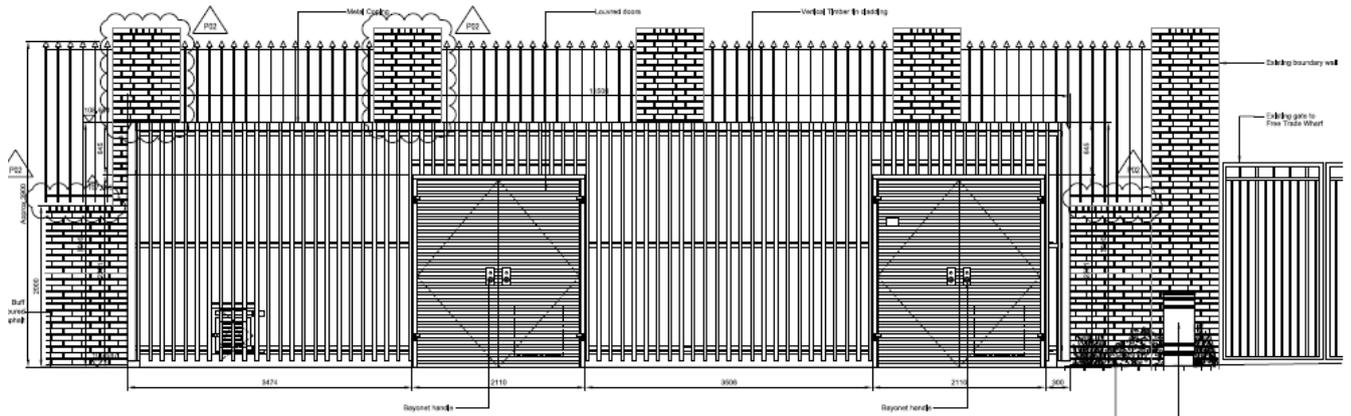
Location

King Edward Memorial Park Foreshore

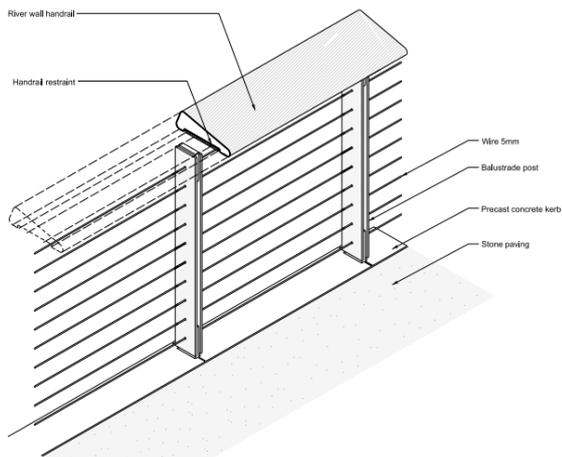
London Borough of Tower Hamlets



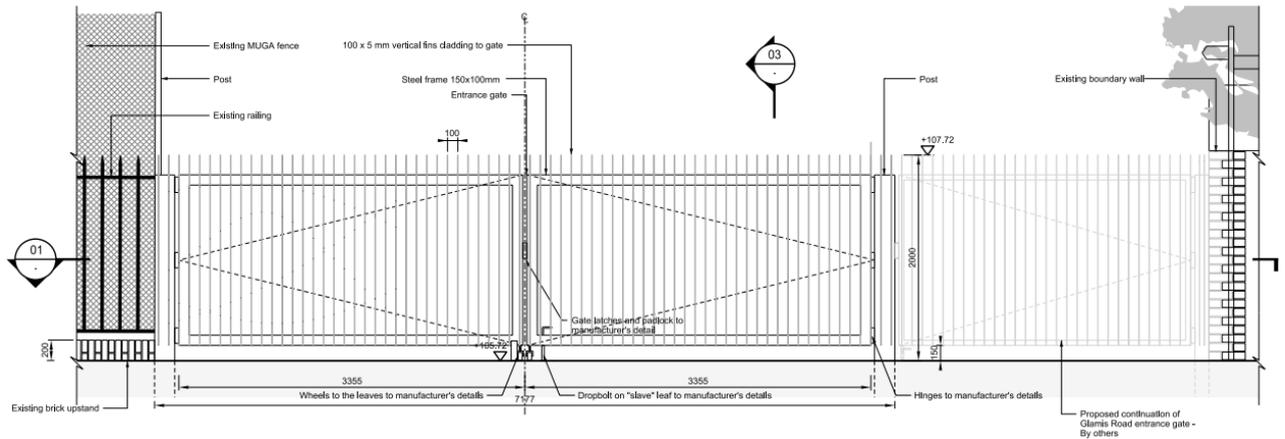
**Proposed western and eastern mound isometrics**



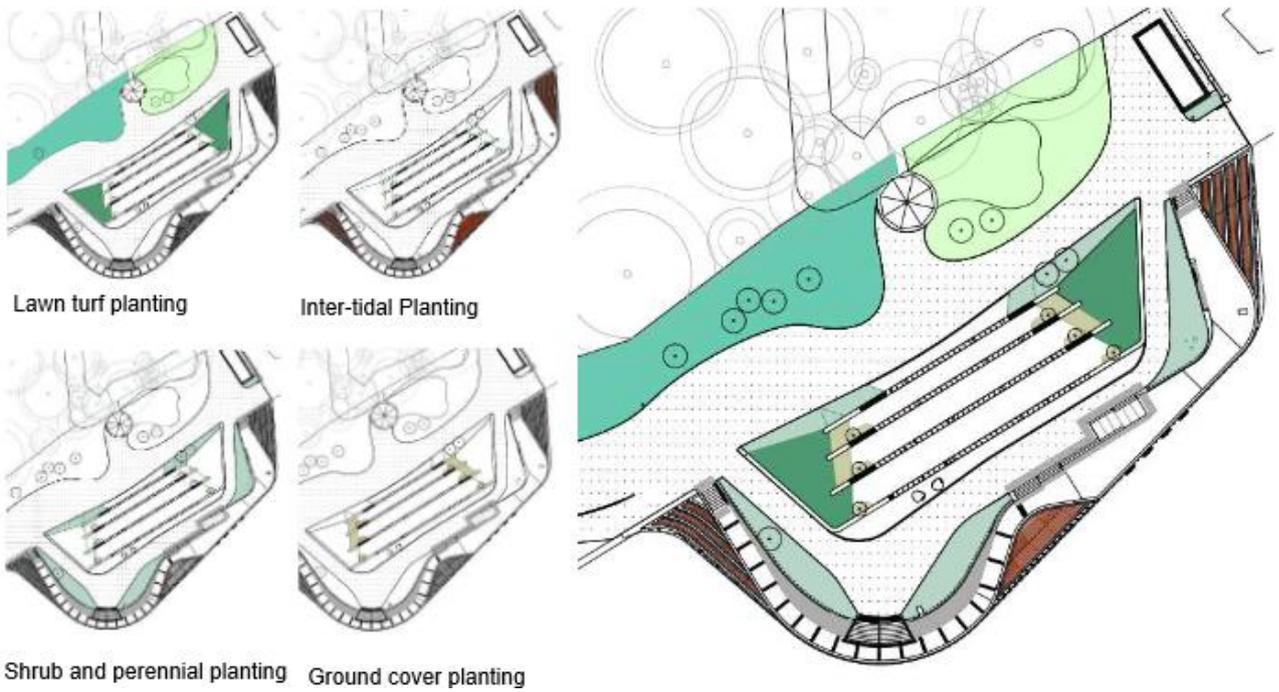
**Proposed kiosk front elevation**



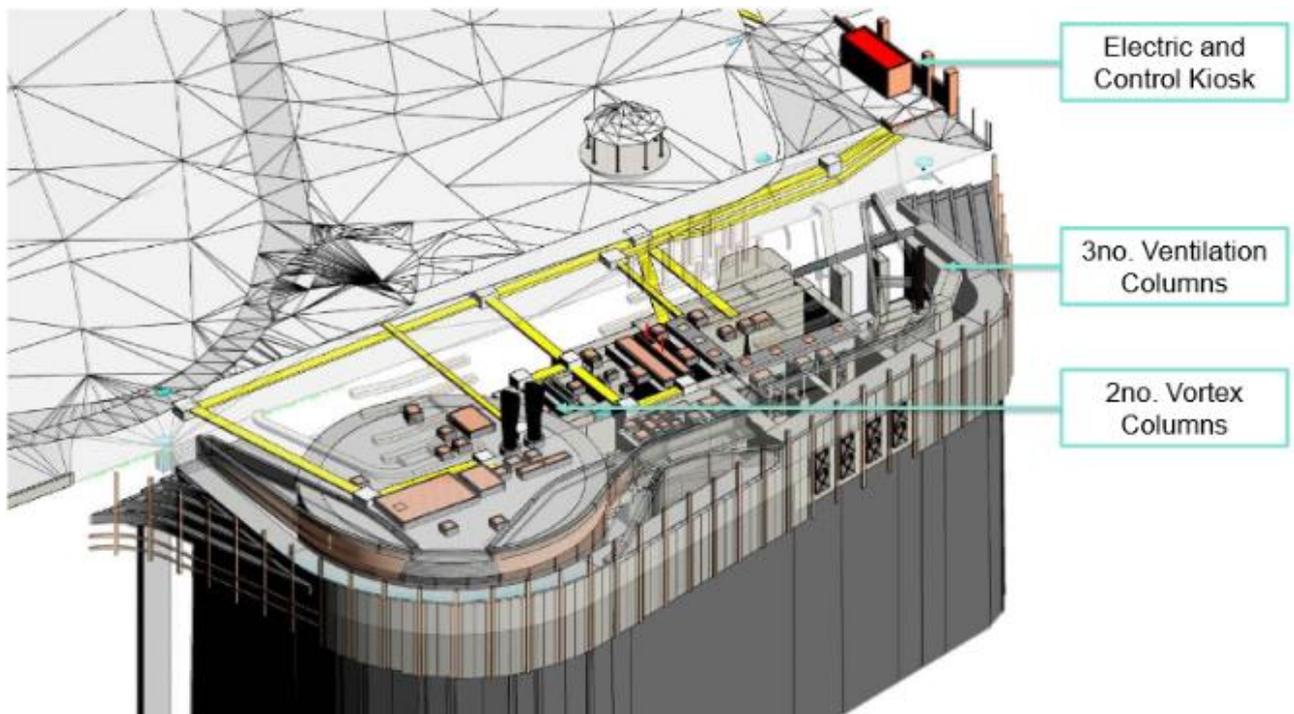
**Proposed balustrade isometric**



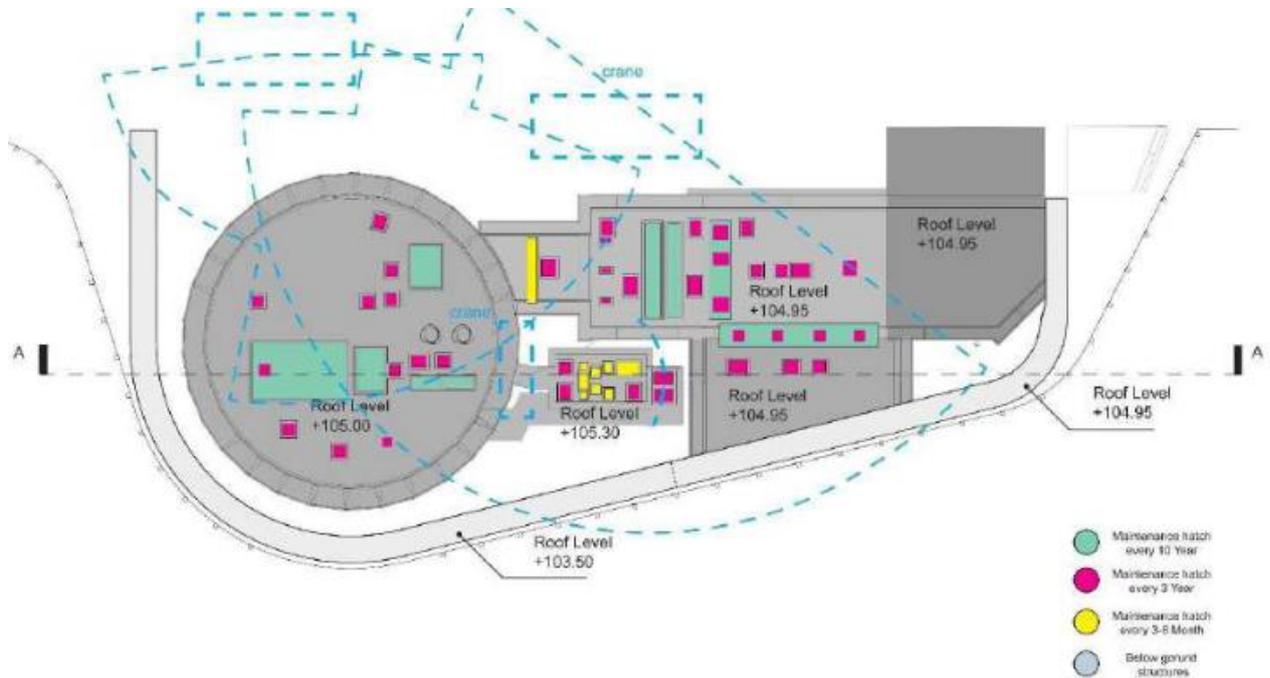
**Proposed Glamis Road gate west elevation**



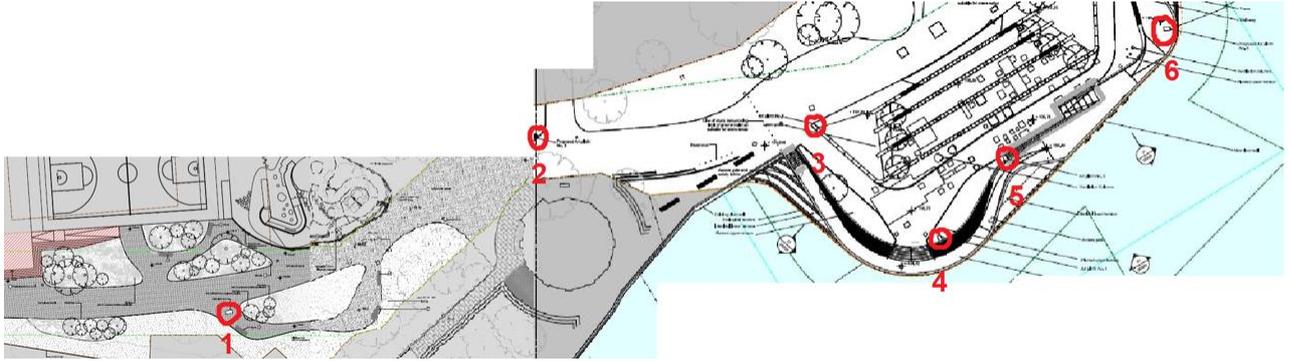
**Proposed planting strategy on and around KEMPF structure**



Proposed illustrative isometric view diagram showing location of functionally required infrastructure (submitted as part of this current application)



Proposed plan showing required access hatches for maintenance



**Proposed locations of art plinths**



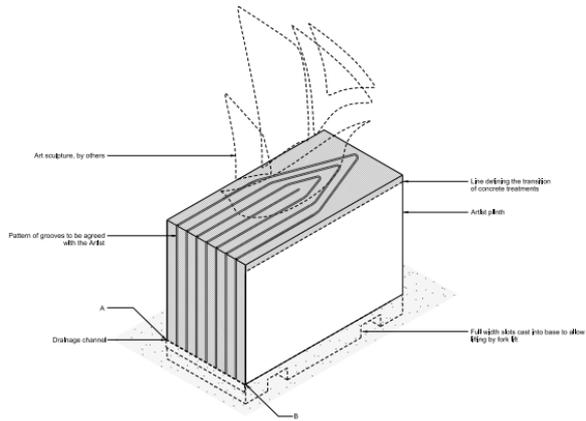
**Proposed illustrative CGI perspective view of the public realm looking eastwards**



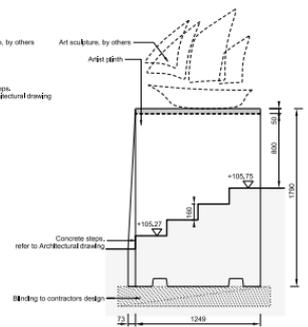
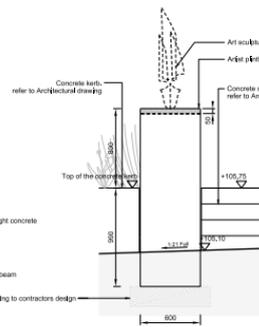
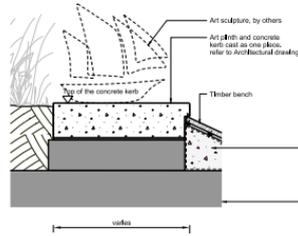
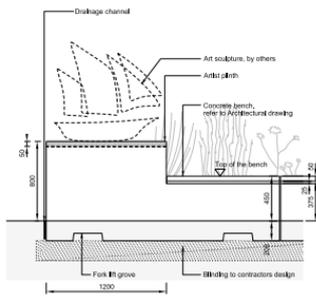
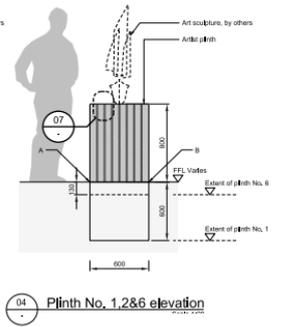
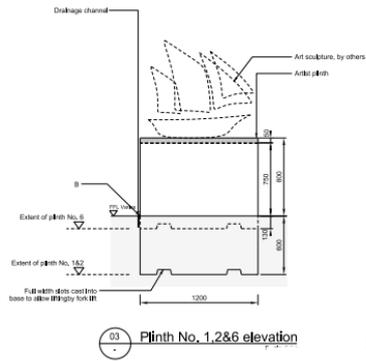
**Proposed illustrative CGI perspective view of kiosk looking eastwards**



**Proposed illustrative CGI bird's eye perspective view of the western intertidal terrace from the west**



01 Plinth No. 6 axo (No. 1&2 similar)



**Proposed indicative isometric and elevations of artwork**